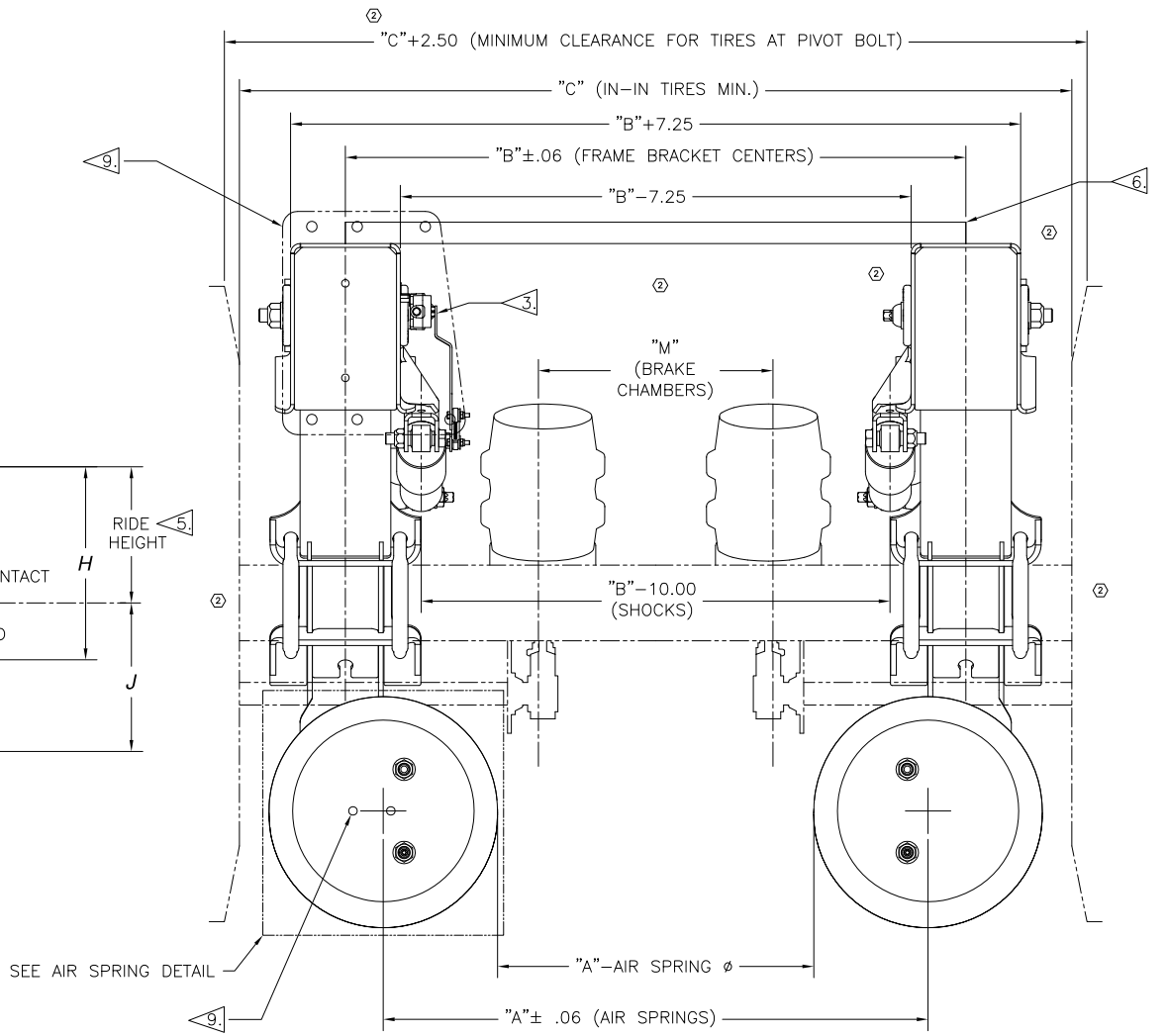
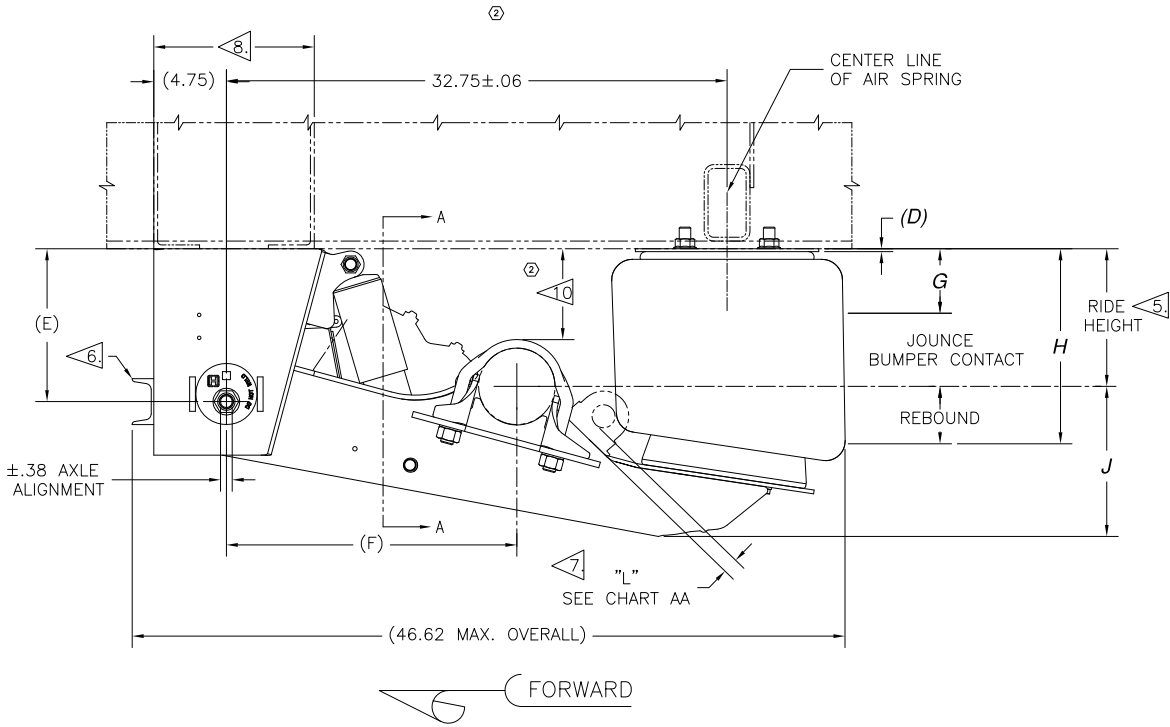
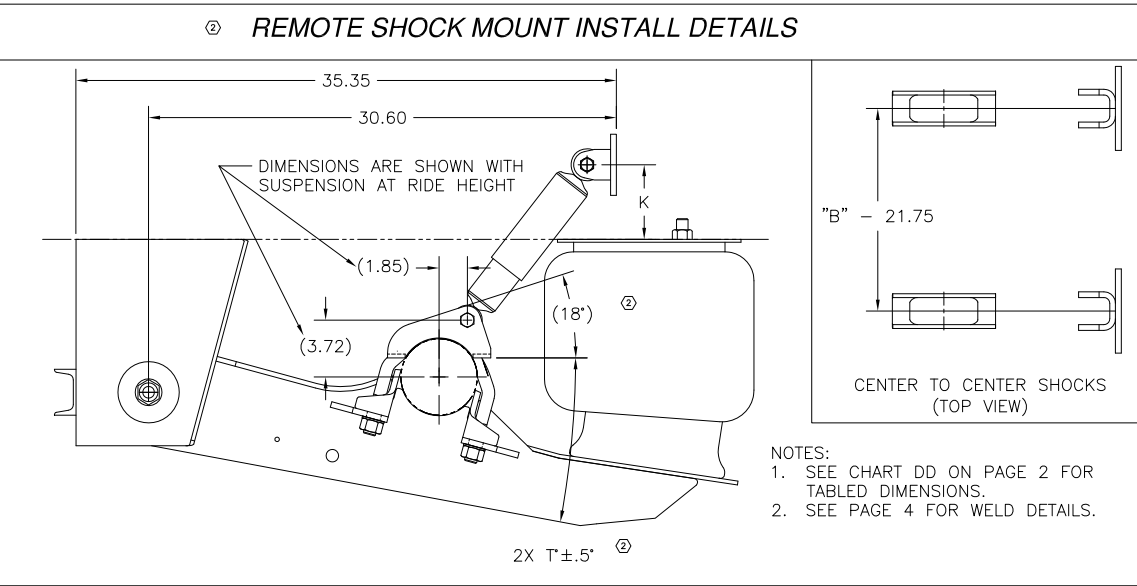
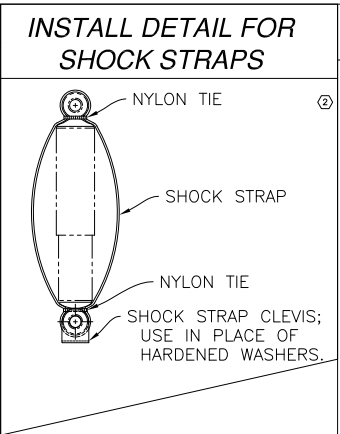
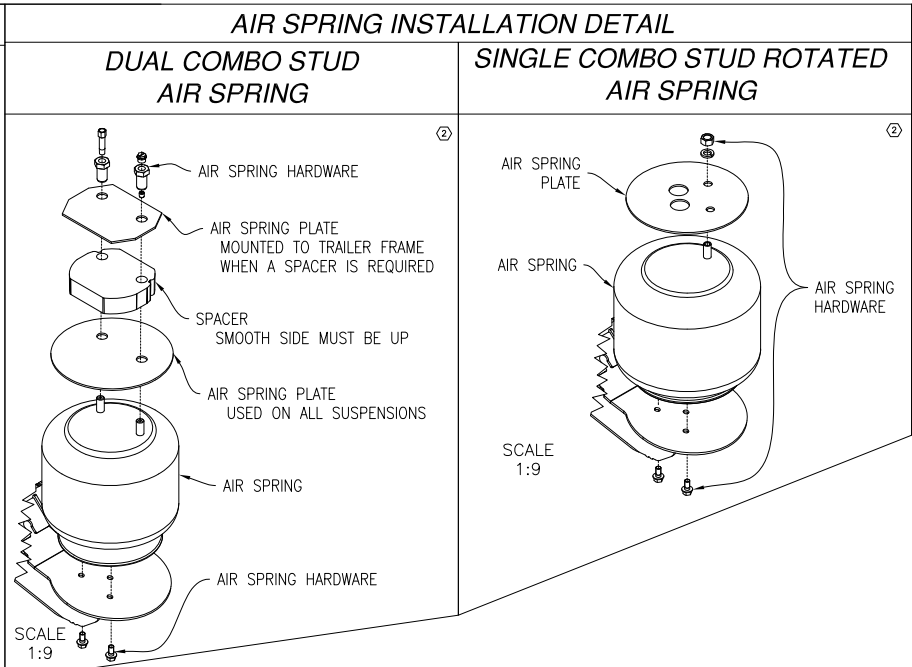
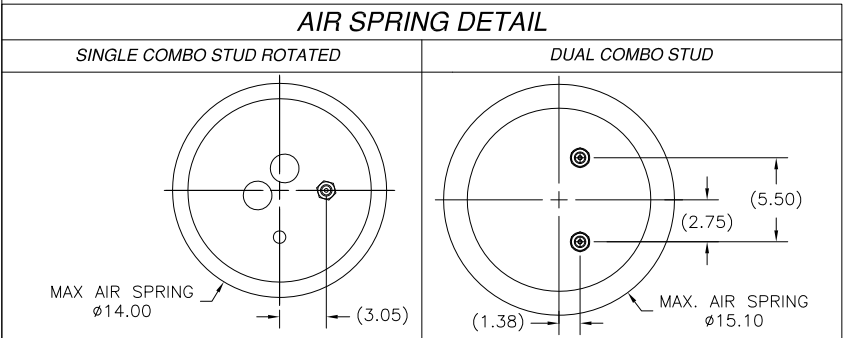


CHART BB
TORQUE SPECIFICATIONS

DESCRIPTION	SIZE	TORQUE (FT LB)
U-BOLTS	7/8-14	475-525
SHOCK BOLTS, UPPER	3/4-10	210-235
AIR SPRING NUTS, UPPER	3/4-16	80-100
AIR SPRING BOLTS, LOWER	1/2-13	40-50






- NOTES: ②
- SEE T44004 FOR INFORMATION ON ASSEMBLY, WELDING PROCEDURE, AND ALIGNMENT.
 - SEE SERVICE MANUAL FOR INFORMATION CONCERNING MAINTENANCE PROCEDURES.
 - SEE L1182 FOR HEIGHT CONTROL KIT OPTIONS.
 - SEE CHART BB FOR TORQUE SPECIFICATIONS OF FASTENERS.
 - SEE PAGE 2 FOR TABULATED DIMENSIONS AND ALLOWABLE RIDE HEIGHT RANGES.
 - SEE PAGE 3 FOR C-CHANNEL AND FRAME BRACKET BRACE MOUNTING REQUIREMENTS.
 - RECOMMENDED BRAKE CAMSHAFT AND BRAKE CHAMBER LOCATIONS LISTED IN CHART AA ON PAGE 2 FOR CAMS REAR. WHEN A CL LIFT KIT IS USED SEE PG 2 FOR S-CAMS FORWARD DETAILS.
 - SEE PAGE 3 AND 4 FOR FRAME BRACKET CROSSMEMBER LOCATIONS AND MOUNTING DETAILS. ACTUAL SIZE AND SHAPE MAY VARY WITH TRAILER DESIGN. IT IS THE RESPONSIBILITY OF THE INSTALLER TO ENSURE PROPER STRUCTURAL ADEQUACY OF TRAILER FRAME AND CROSSMEMBERS.
 - SEE PAGE 5 FOR BOLT-ON MOUNTING PATTERN.
 - DO NOT ROUTE ITEMS THROUGH THIS AREA DUE TO MINIMAL BEAM TO FRAME CLEARANCE.
 - SUSPENSION CAPACITY: 30,000 LBS. @ GROUND. SUSPENSION CAPACITY ONLY INCLUDES THE SUSPENSION AS SUPPLIED BY HENDRICKSON. CAPACITY RATINGS OF AXLE, BRAKES, TIRES, WHEELS, ETC. MAY LIMIT THE OVERALL CAPACITY OF THE SYSTEM.















CAUTION

CLEARANCE SPECIFICATIONS:

- 1.0 INCH MINIMUM REQUIRED BETWEEN TOP OF TIRE AND BOTTOM OF TRAILER STRUCTURE WHEN AXLE IS AT FULL JOUNCE.
- 2.0 INCHES MINIMUM REQUIRED BETWEEN INSIDE OF TIRE AND TRAILER STRUCTURE FOR LATERAL MOVEMENT.
- 1.0 INCH MINIMUM CLEARANCE MUST BE MAINTAINED AROUND AIR SPRING WHEN IT IS AT MAXIMUM DIAMETER.

STANDARD TRAVEL HT300US												
	RIDE HEIGHT			BUMPER CONTACT	D	E	F			 RIDE HEIGHT TOLERANCE LIMITS		SUSP. WEIGHT (LB)
		JOUNCE	REBOUND					G	H	MIN.	MAX.	
WELD—ON STANDARD SHOCK MOUNT	5.5	Ⓢ 2.1	4.7	1.4	.19	8.0	18.86	Ⓢ 3.4	10.2	5.5	Ⓢ 7.25	389
	6.5	Ⓢ 3.1	3.7	2.4	.19	8.0	18.97	Ⓢ 3.4	10.2	5.5	Ⓢ 7.25	389
	7.5	Ⓢ 3.2	6.0	Ⓢ 2.7	.19	10.0	18.86	Ⓢ 4.3	13.5	7.5	10.5	406
	9.0	Ⓢ 4.7	4.5	Ⓢ 4.2	.19	10.0	19.00	Ⓢ 4.3	13.5	7.5	10.5	406
	12.0	4.9	4.0	4.3	3.50	12.0	19.03	7.1	16.0	Ⓢ 10.75	13.0	427
	14.0	5.0	4.0	4.4	5.31	14.0	19.03	9.0	18.0	12.5	15.0	448
	17.0	5.5	3.5	5.0	7.31	17.0	19.03	11.5	20.5	15.0	17.5	474
BOLT—ON STANDARD SHOCK MOUNT	5.5	1.8	5.1	1.2	.25	8.38	18.81	3.7	10.6	5.5	Ⓢ 7.5	419
	6.5	2.8	4.1	2.2	.25	8.38	18.93	3.7	10.6	Ⓢ 5.5	Ⓢ 7.5	419
	7.5	2.9	6.4	2.4	.25	10.38	18.81	4.6	13.9	7.5	Ⓢ 11.0	437
	9.0	4.4	4.9	3.9	.25	10.38	18.98	4.6	13.9	7.5	Ⓢ 11.0	437
	12.0	4.6	4.2	4.1	3.56	12.5	19.02	7.4	16.2	Ⓢ 11.0	Ⓢ 13.25	467
WELD—ON REMOTE SHOCK MOUNT	5.5	2.0	4.0	1.4	.19	8.0	18.86	3.5	9.5	5.5	6.5	391
	6.5	3.0	3.0	2.4	.19	8.0	18.97	3.5	9.5	Ⓢ 5.5	6.5	391
	7.5	3.1	4.4	2.6	.19	10.0	18.86	4.4	11.9	Ⓢ 7.5	9.0	408
	9.0	4.6	2.9	4.1	.19	10.0	19.00	4.4	11.9	Ⓢ 7.5	9.0	408
	12.0	4.9	2.7	4.3	3.50	12.0	19.03	7.1	14.7	Ⓢ 10.75	12.0	429
	14.0	5.0	3.5	4.4	5.31	14.0	19.03	9.0	17.5	Ⓢ 12.5	14.5	453
17.0	5.5	3.3	5.0	7.31	17.0	19.03	11.5	20.3	Ⓢ 15.0	Ⓢ 17.25	479	

LIMITED JOUNCE TRAVEL HT300US												
	RIDE HEIGHT			BUMPER CONTACT	D	E	F			RIDE HEIGHT TOLERANCE LIMITS 		SUSP. WEIGHT (LB)
		JOUNCE	REBOUND					MIN.	MAX.			
WELD—ON STANDARD SHOCK MOUNT	6.5	2.5	3.7	1.9	1.13	8.0	18.97	4.0	10.2	6.5	② 7.25	397
	7.5	2.6	6.0	2.0	1.13	10.0	18.86	4.9	13.5	7.5	10.5	414
	9.0	3.5	4.5	2.9	2.25	10.0	19.00	5.5	13.5	② 9.0	10.5	417
	12.0	3.9	4.0	3.3	5.31	12.0	19.03	8.1	16.0	② 11.75	13.0	434
	14.0	3.9	4.0	3.3	7.31	14.0	19.03	10.1	18.0	② 13.75	15.0	456
BOLT—ON STANDARD SHOCK MOUNT	17.0	4.5	3.5	3.9	9.25	17.0	19.03	12.5	20.5	② 16.0	17.5	481
	6.5	2.3	4.1	1.7	1.19	8.38	18.93	4.2	10.6	6.5	② 7.5	426
	7.5	2.4	6.4	1.8	1.19	10.38	18.81	5.1	13.9	7.5	② 11.0	445
	9.0	3.3	4.9	2.7	2.31	10.38	18.98	5.7	13.9	② 9.0	② 11.0	446
	6.5	2.5	3.0	1.9	1.13	8.0	18.97	4.0	9.5	6.5	6.5	400
WELD—ON REMOTE SHOCK MOUNT	7.5	2.6	② 4.4	2.0	1.13	10.0	18.86	4.9	② 11.9	7.5	② 8.75	417
	9.0	3.5	2.9	2.9	2.25	10.0	19.00	5.5	11.9	② 9.0	9.0	419
	12.0	3.9	2.7	3.3	5.31	12.0	19.03	8.1	14.7	② 11.75	12.0	436
	14.0	3.9	3.4	3.3	7.31	14.0	19.03	10.1	17.4	② 13.75	② 14.5	461
	17.0	4.5	3.3	3.9	9.25	17.0	19.03	12.5	20.3	② 16.0	② 17.25	487

EXTENDED REBOUND TRAVEL HT300US												
	RIDE HEIGHT			BUMPER CONTACT	<i>D</i>	<i>E</i>	<i>F</i>			 RIDE HEIGHT TOLERANCE LIMITS		SUSP. WEIGHT (LB)
		JOUNCE	REBOUND					<i>G</i>	<i>H</i>	MIN.	MAX.	
WELD-ON STANDARD SHOCK MOUNT	6.5	3.0	5.1	2.4	.19	8.0	18.97	3.5	11.6	6.5		389
WELD-ON REMOTE SHOCK MOUNT	6.5	3.0	5.2	2.4	.19	8.0	18.97	3.5	11.7	6.5		395

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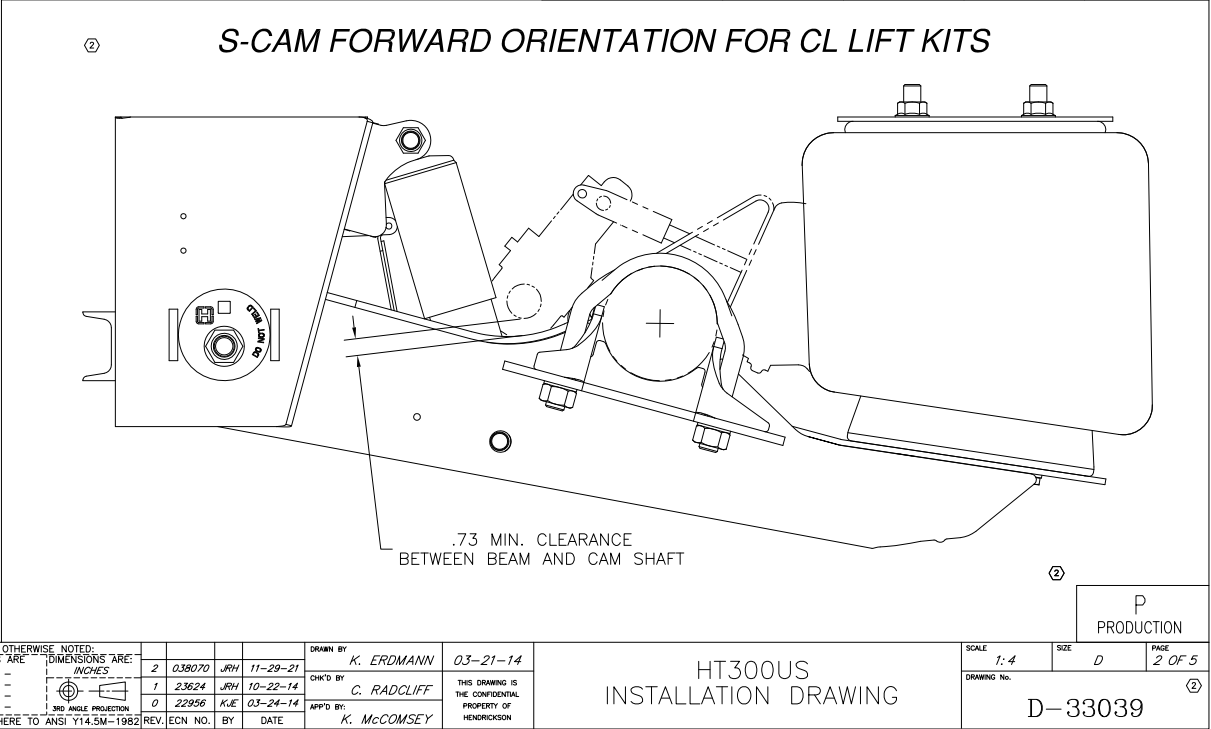
CHART CC		
TRAILER WIDTH (IN)	96.0	102.0
DIMENSION "A" (IN)	30.00	36.00
DIMENSION "B" (IN)	35.00	41.00
DIMENSION "C" (IN)	46.50	52.50
DIMENSION "M" (IN)	9.5 MAX.	15.5 MAX.

- NOTES:
- SUSPENSION WEIGHT INCLUDES SUSPENSION COMPONENTS ONLY. WEIGHT REFLECTS:
NO SHOCK STRAP KIT
WITH C-CHANNEL (LESS 14 LBS. WITHOUT C-CHANNEL)
STANDARD AIR SPRING MOUNTING\SPACER
 - JOUNCE AND REBOUND DIMENSIONS CHANGE AS THE RIDE HEIGHT CHANGES FROM THE NOMINAL POSITION.
 - DIMENSIONS "G" & "H" WILL REMAIN CONSTANT REGARDLESS OF RIDE HEIGHT VARIATION FROM NOMINAL POSITION.
RIDE HEIGHT – JOUNCE = "G"
RIDE HEIGHT + REBOUND = "H"
 - FOR OPTIMUM SUSPENSION PERFORMANCE ALL SUSPENSIONS ON A TRAILER SHOULD BE AT DESIGNED RIDE HEIGHT WHEN THE TRAILER IS LOADED. TRAILERS WITH FLEXIBLE FRAMES REQUIRE SPECIAL ATTENTION TO MAKE SURE THEY OPERATE WITHIN THE RIDE HEIGHT TOLERANCE LIMITS, BOTH EMPTY AND LOADED. OPERATION OUTSIDE OF THE MAXIMUM AND/OR MINIMUM RIDE HEIGHT TOLERANCE LIMITS CAN REDUCE RIDE QUALITY AND SHORTEN SUSPENSION LIFE.
 - VARIATION IN RIDE HEIGHT BETWEEN LIKE SUSPENSIONS MAY RESULT IN UNEQUAL LOADING OF THE AXLES.
 - RECOMMENDED BRAKE CAMSHAFT AND BRAKE CHAMBER LOCATIONS LISTED IN CHART AA ARE FOR REFERENCE ONLY AND MAY VARY SLIGHTLY BASED ON AXLE MANUFACTURERS RECOMMENDATIONS.
 - RECOMMENDED BRAKE S-CAM "L" POSITION OF 1.00" IS SHOWN TO PROVIDE CLEARANCE AROUND THE BRAKE CHAMBER FOR ACCESS TO THE LOWER SHOCK BOLT WITH AN IMPACT WRENCH. THE S-CAM CANNOT BE POSITIONED FOR SHOCK BOLT CLEARANCE ON 5.5" AND 6.5" RIDE HEIGHTS DUE TO INTERFERENCE BETWEEN THE BRAKE CHAMBER AND THE TRAILER FRAME AT FULL JOUNCE. SEE PAGE 3 FOR S-CAM ORIENTATION WHEN USING CL LIFT KITS.

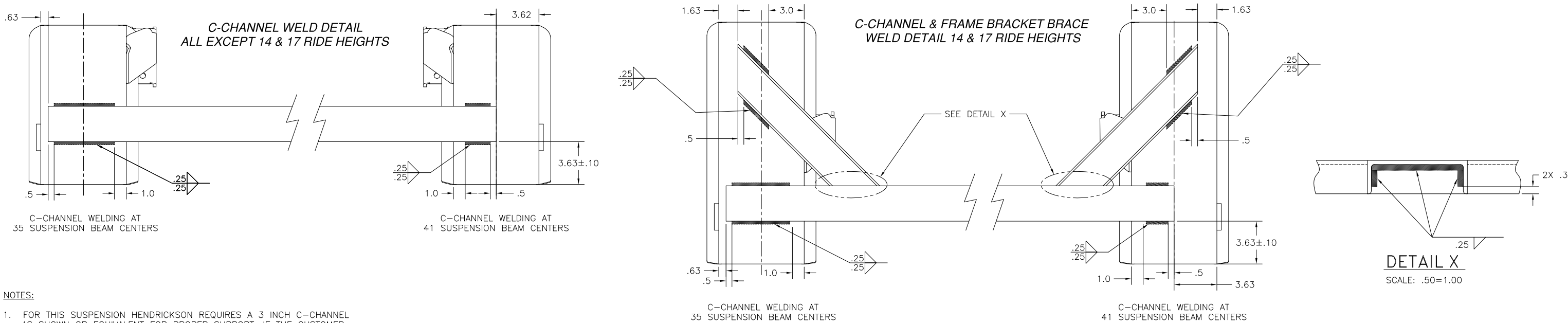
GROUND CLEARANCE	
TO CALCULATE GROUND CLEARANCE, SUBTRACT "J" FROM LOADED TIRE RADIUS.	
RIDE HEIGHT	J
5.5	9.05
6.5	9.57
7.5	9.05
9.0	9.81
12.0	10.29
14.0	10.29
17.0	10.29

REMOTE SHOCK MOUNT				
CHART DD				
FRAME BRACKET MOUNTING	TRAVEL	RIDE HEIGHT	DIMENSION	
			K	T
WELD-ON	STANDARD	5.5	5.13	6.0"
		6.5		9.0"
	STANDARD -OR- LIMITED JOUNCE	7.5	5.75	6.0"
		9.0		10.6"
		12.0	2.88	13.6"
		14.0	4.88	13.6"
		17.0	2.00	13.6"
	EXT. REBOUND	6.5	10.50	9.0"

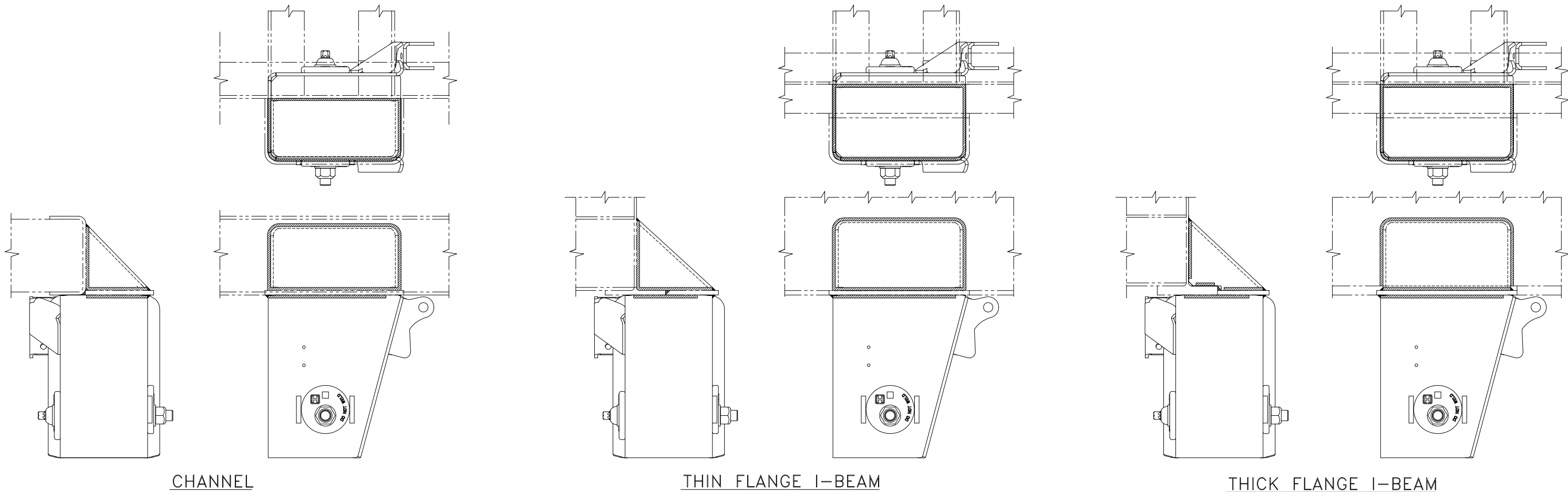
S-CAM REAR ORIENTATION (STD.)				
	CHART AA			
	20", 22", 24" TIRES		19.5" TIRES	
	RIDE HEIGHT	DIM "L"	RIDE HEIGHT	DIM "L"
ALL TRAVELS STD SHOCK MOUNT WELD-ON AND BOLT-ON	5.5	(1.00)	5.5	(1.00)
	6.5	(1.00)	6.5	(1.00)
	7.5	(1.00)	7.5	(1.00)
	9.0	(1.00)	9.0	(1.00)
	12.0	(1.00)	12.0	(1.00)
	14.0	(1.00)	14.0	(1.00)
	17.0	(1.00)	17.0	(1.00)
ALL TRAVELS REMOTE SHOCK MOUNT WELD-ON	5.5	(2.25)	5.5	(1.94)
	6.5	(2.49)	6.5	(2.16)
	7.5	(2.28)	7.5	(1.97)
	9.0	(2.63)	9.0	(2.30)
	12.0	(2.85)	12.0	(2.50)
	14.0	(2.85)	14.0	(2.50)
	17.0	(2.85)	17.0	(2.50)



② LATERAL FRAME BRACKET SUPPORT INSTALL DETAIL



② SUGGESTED METHOD OF SUPPORTING FRAME BRACKET OVERHANG



②

P
PRODUCTION

②

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HENDRICKSON

TRAILER COMMERCIAL VEHICLE SYSTEMS
2070 INDUSTRIAL PLACE S.E., CANTON, OH 44707-2600 U.S.A.

UNLESS OTHERWISE NOTED:
TOLERANCES ARE: DIMENSIONS ARE:
X: ± .005 INCHES
XX: ± .010 INCHES
XXX: ± .015 INCHES
ANGULAR: ± .5°
3RD ANGLE PROJECTION
DIMENSIONS ADHERE TO ANSI Y14.5M-1982

2 038070 JRM 11-29-21
1 23624 JRM 10-22-14
0 22956 KLE 03-24-14

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CHK'D BY: C. RADCLIFF
APP'D BY: K. McCOMSEY

03-21-14
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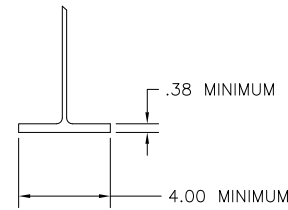
SCALE: 1:4
SIZE: D
PAGE: 3 OF 5
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SUGGESTED WELD-ON ATTACHMENT DETAIL

FRAME BRACKET
WELD-ON WINGLESS

AIR SPRING PLATE
DUAL COMBO STUD



VIEW AA

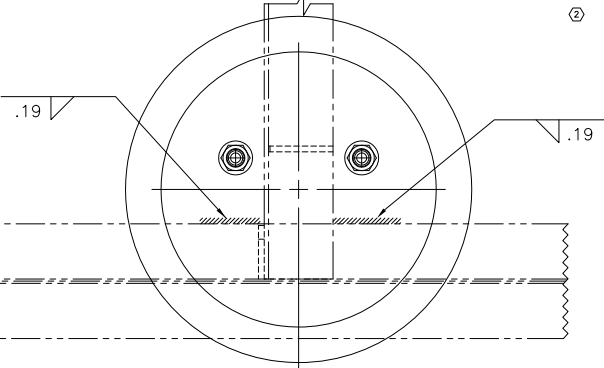
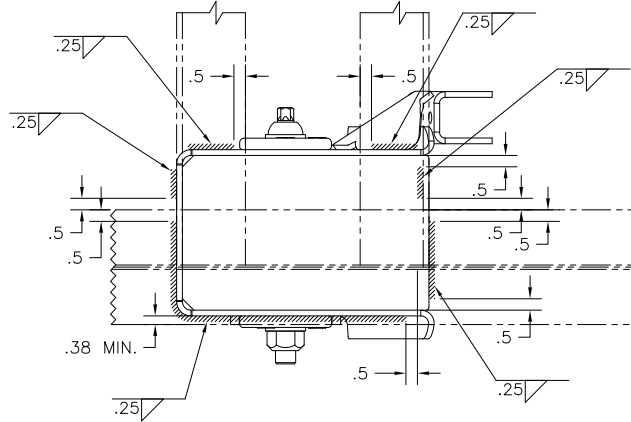
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MAIN BEAM BOTTOM FLANGE
DIMENSIONAL REQUIREMENTS

GUSSET
(FULL HEIGHT RECOMMENDED)

EXTERNAL WEB STIFFENER (OPTIONAL)
ALIGNED WITH WEB OF EACH CROSSMEMBER
RECOMMENDED IF INTERNAL GUSSETS
ARE OMITTED OR NOT FULL-HEIGHT

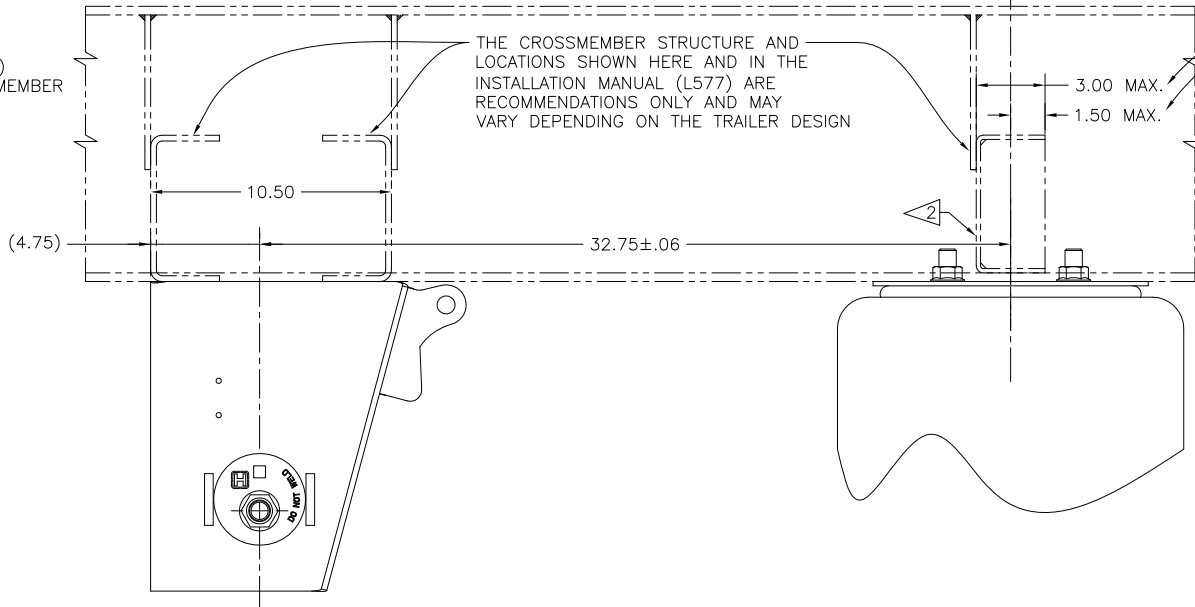
VIEW AA



MOUNTING PLATE
SHAPE WHEN
SPACER IS
REQUIRED

NECESSARY TO PROVIDE
CLEARANCE FOR MOUNTING
NUTS AND SOCKET WRENCH

INSTALLER TO SUPPLY SPACER
BETWEEN TOP OF AIR SPRING
MOUNTING PLATE AND
UNDERSIDE OF CROSSMEMBER



THE CROSSMEMBER STRUCTURE AND
LOCATIONS SHOWN HERE AND IN THE
INSTALLATION MANUAL (L577) ARE
RECOMMENDATIONS ONLY AND MAY
VARY DEPENDING ON THE TRAILER DESIGN

2

SINGLE COMBO STUD ROTATED
DETAIL

LOCATE AIR SPRING
CROSSMEMBER AS SHOWN
AND ADD GUSSET FOR
ADDITIONAL SUPPORT.

1.25 MIN

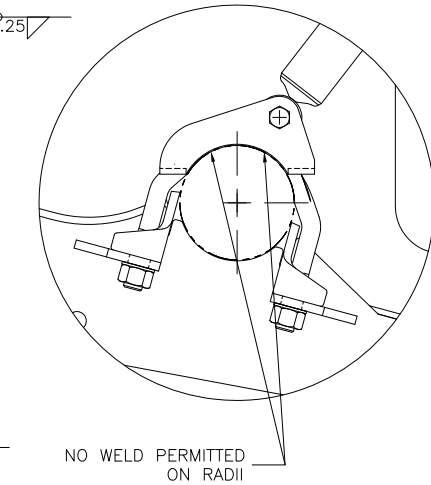
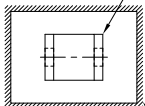
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AIR SPRING MAY BE ROTATED TO ORIENTATE COMBO
STUD ACCOMMODATE INDIVIDUAL FRAME REQUIREMENTS,
PROVIDED CUSTOMER MAINTAINS ADEQUATE SUPPORT.
SEE L64 FOR ADDITIONAL INFORMATION.

REMOTE SHOCK MOUNT WELDING
DETAIL

2

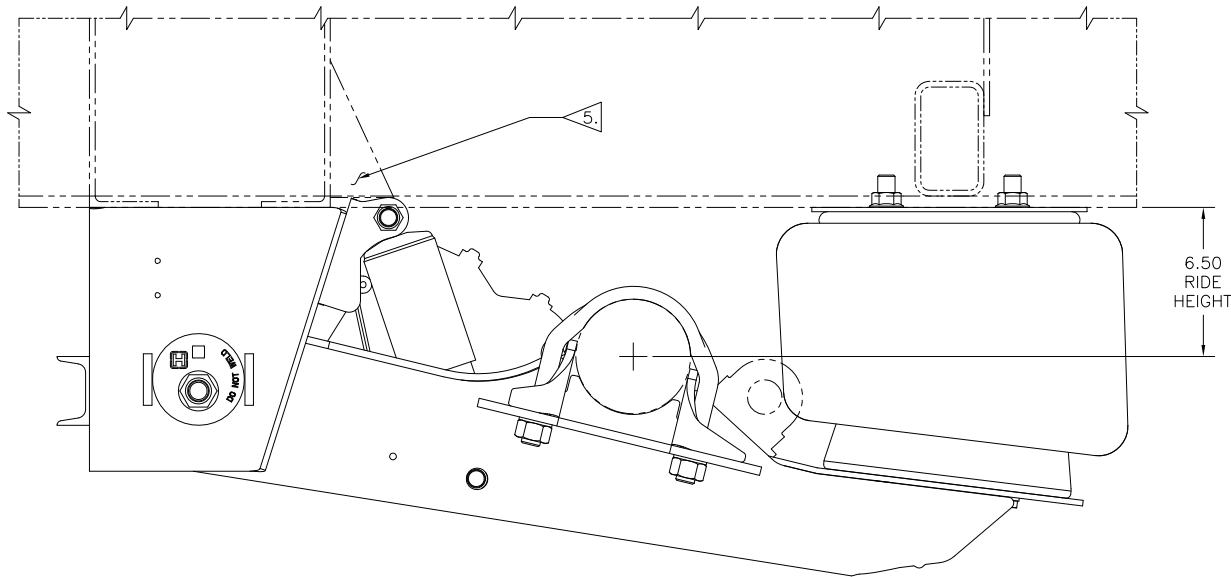
CROSSMEMBER AND SHOCK
ATTACHMENT AREA MUST BE
CAPABLE OF WITHSTANDING UP
TO 8,500 LB OF FORCE PER
SIDE WHEN SUSPENSION IS IN
THE FULL REBOUND POSITION



2

P
PRODUCTION

6.5 RIDE HEIGHT EXTENDED REBOUND
WELD-ON MOUNTING INFORMATION



NOTES:

1. // PATTERN DENOTES WELD PLACEMENT
2. CROSSMEMBER AND/OR GUSSETS MUST ADEQUATELY SUPPORT UPPER AIR SPRING PLATE. 20,000 LBS. CAN BE EXERTED THROUGH AIR SPRING BUMPER
3. FOR FRAME BRACKET OVERHANG SEE PAGE 6 FOR SUGGESTED METHOD OF SUPPORT
- 2 4. SEE L64 TRAILER SUSPENSION SYSTEMS WELDING PROCEDURES FOR ADDITIONAL INFORMATION ON FRAME BRACKET, CROSS MEMBER, UPPER SHOCK BRACKET AND AIR SPRING MOUNT WELDING PROCEDURES.
- 2 5. BECAUSE THE SHOCK IS NOT FULLY SUPPORTED BY THE FRAME BRACKET ASSEMBLY, THE INSTALLER MUST REINFORCE THE CLEVIS. TO THE TRAILER STRUCTURE. 8,000 LB. IS NORMALLY THE MAXIMUM LOAD AT THE SHOCK.

HENDRICKSON

TRAILER COMMERCIAL VEHICLE SYSTEMS
2070 INDUSTRIAL PLACE S.E., CANTON, OH 44707-2600 U.S.A.

UNLESS OTHERWISE NOTED:		DIMENSIONS ARE:	
TOLERANCES ARE:		INCHES	
X: ±	2	0.38070	JRH 11-29-21
XX: ±	1	2.3624	JRH 10-22-14
XXX: ±	0	22956	KLE 03-24-14
ANGULAR: ±			

3RD ANGLE PROJECTION	REV.	ECN NO.	BY	DATE
----------------------	------	---------	----	------

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CHK'D BY	C. RADCLIFF	
APP'D BY:	K. McCOMSEY	

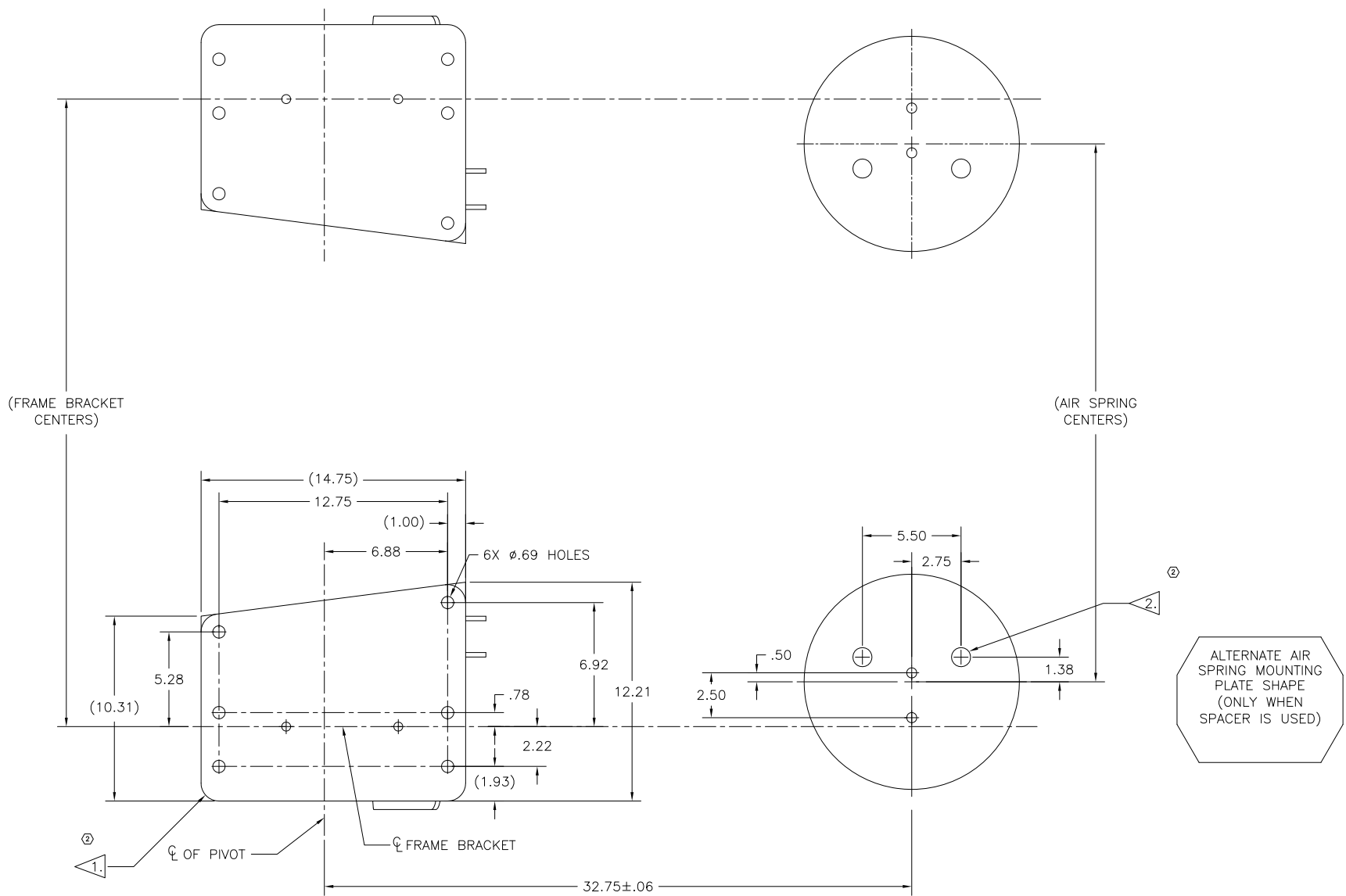
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BOLT-ON MOUNTING PATTERN & INSTALLATION DETAIL



NOTES:

- ① HENDRICKSON STANDARD COMPACT BOLT PATTERN SHOWN.
- ② DUAL COMBO STUD AIR SPRING IS ONLY OPTION FOR BOLT-ON

②

P
PRODUCTION

②

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UNLESS OTHERWISE NOTED:
TOLERANCES ARE: DIMENSIONS ARE:
X: ± .005 INCHES
Y: ± .005 INCHES
Z: ± .005 INCHES
ANGULAR: ± .5°
3RD ANGLE PROJECTION
DIMENSIONS ADHERE TO ANSI Y14.5M-1982

2 038070 JRM 11-29-21
1 23624 JRM 10-22-14
0 22956 KLE 03-24-14

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